

City of Virginia Beach Laskin Road Gateway

ASHE Meeting
September 17, 2013



Project's Goals



- Area designated as a "Strategic Growth Area" (SGA).
- Planning/Economic Development: Improve public infrastructure such that private redevelopment of the area is encouraged and the area becomes a destination point for visitors and shoppers. Make area "Pedestrian Friendly".



- Public Works/Traffic Engineering: Maintain or improve traffic capacity within the Gateway network while improving pedestrian access.



The Laskin Road Gateway - Before



Project Vision-Streetscape



Existing Conditions

- Narrow sidewalk
- No Pedestrian Lighting
- No Benches, Bike Racks

The "Vision"

- Wide Sidewalks
- More green space
- Street Furniture
- LED Pedestrian Lighting



Project Vision-Overhead Utilities



Existing Conditions

- Most dry utilities overhead

The "Vision"

- Relocate all overhead utilities underground



Project Vision-Traffic



Existing Conditions

- "Nose-in" parking
- 5 lanes including Two-Way Left Turn Lane

The "Vision"

- 2 lanes and parallel parking



The New Laskin Road Gateway



9/23/2013

7

The Gateway Traffic Network



LASKIN ROAD GATEWAY PHASE 1-A
CIP #2-143
ALTERNATIVE B2 (REVISED)
CITIZENS INFORMATION MEETING
JUNE 10, 2010

An aerial photograph of a commercial district in New Rochelle, New York. The image shows a large, paved parking lot filled with numerous cars. Several commercial buildings are visible, including a large, dark-roofed building and a smaller building with a blue roof. A curved road with multiple lanes runs along the right side of the parking lot. In the bottom right corner, a small boat is visible in the water. The overall scene depicts a busy urban environment.

SCALE 1" = 20'

LEGEND

- SUBMIT AREA TRAFFIC SIGNAL POLE
- AREA WITH TRAFFIC SIGNAL POLE
- PED/BICYCLE SIGNAL POLE
- TORNADE SHELTER LIGHT
- PED/BICYCLE STREET LIGHT
- PARK BENCH
- BICYCLE RACKS
- STREET TREE
- GROUND COVER PLANTING

BEACH CENTER MIXED USE DEVELOPMENT (BY OTHERS)

LASKIN ROAD GATEWAY PHASE 1-2 CIP #2-143

PHASE 3

CITIZENS INFORMATION MEETING JUNE 10, 2010

Final Phase – Complete Traffic Network and Undergrounding of Utilities



Major Infrastructure Improvements

Water/Sewer



Major Infrastructure Improvements

Storm Sewer



Major Infrastructure Improvements

Ductbank



Major Infrastructure Improvements

New Bridge



Major Infrastructure Improvements

New Bulkhead



Major Infrastructure Improvements

Twin 72-Inch Box Culvert Extension



Major Infrastructure Improvements

Roadway And Paver Intersections



Major Infrastructure Improvements

Pedestrian and Street Lighting



Major Infrastructure Improvements

Wide Sidewalks



Major Infrastructure Improvements

New Intersection – Modern Roundabout



Major Infrastructure Improvements

New Traffic Signals

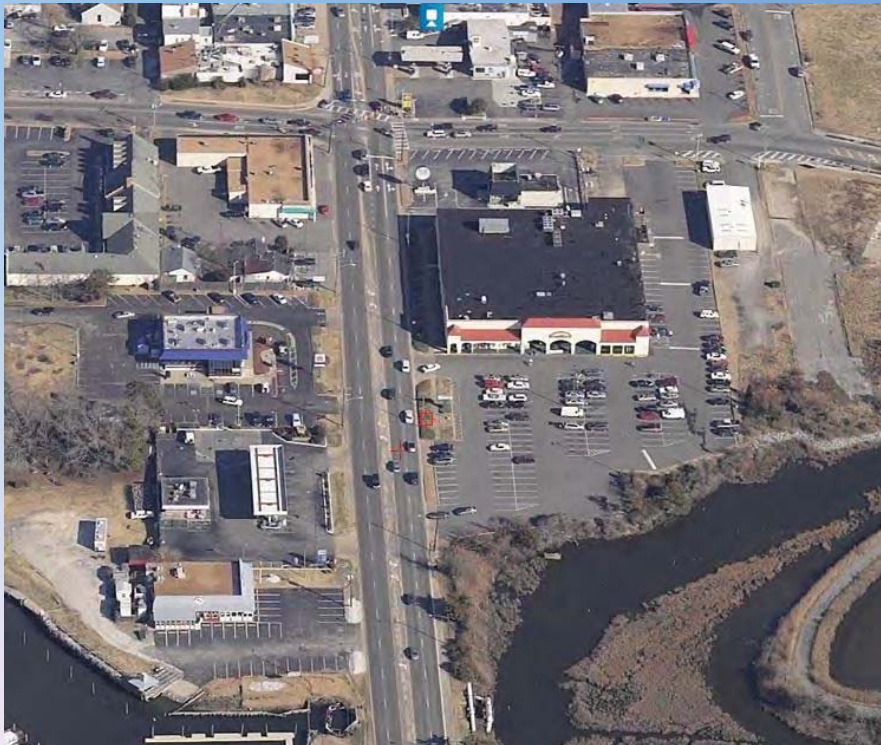


Major Infrastructure Improvements

New Park – Gateway Park










Laskin Road- 2004





Initial 30th/31st Street Traffic Models

	Laskin_Existing	SY7 File
	Laskin_five_Lane_Two_Way	SY7 File
	Laskin_Four_Lane_Two_Way	SY7 File
	Laskin_Three_Lane_One_Way	SY7 File
	Laskin_Three_Lane_Two_Way	SY7 File
	Laskin_Two_Lane_One_Way	SY7 File
	Laskin_Two_Lane_One_Way_parking	SY7 File



Preferred Option – Unbalanced Pair



Laskin Road - 2008



Laskin Road – later in 2008



Birth of 32nd Street – 2007/08



Initial Thoughts on 30th/32nd Pair



- Basic conclusions of the 30th/31st Street traffic study would transfer to 30th/32nd Street concept; however....
- **32nd Street would be required to carry 90 % of North End traffic.**
- Concerns with 3 lane roundabout if 30th/32nd were not one-way.

Early Concept for Roundabout



Laskin Road Gateway - Laskin Road/30th Street/32nd Street Intersection Option Matrix

6/4/2009



OPTIONS		LANEAGE & GEOMETRY	OPERATIONAL CHARACTERISTICS		DESIGN CONSIDERATIONS		
			LOS & Queuing (2030 PM Off-Season)	EVENT TRAFFIC & EVACUATION TRAFFIC	R.O.W. IMPACTS	Construction Cost Differential	Constructability
					ALIGNMENT A		
ONE-WAY OPTION	OPTION 1		Approach LOS: A - B	Most effectively accommodates the free flow loading and unloading traffic, particularly along 30th and 32nd Streets.	Reqd. R/W = 0.79 acres	Base	Laskin Road will be closed off during construction.
	UNSIGNALIZED INTERSECTION		Queuing: 0 - 75 feet		R/W & Total Takes = 1.43 acres		
TWO-WAY OPTIONS	OPTION 2		Approach LOS: A - C	Provides efficient loading and unloading of traffic; however some impedance will occur at the roundabout.	Reqd. R/W = 1.25 acres	Additional \$100,000	Laskin Road will be closed off during construction.
	MODERN ROUNDABOUT		Queuing: 50 - 175 feet		R/W & Total Takes = 1.75 acres		
	OPTION 3		Approach LOS: C - D	Unless manual traffic control is provided, signal will create excessive delay in loading (particularly to 32nd Street) and unloading (particularly from 30th Street)	Reqd. R/W = 1.17 acres	Additional \$350,000	Laskin Road will remain open for most of construction.
	SIGNALIZED INTERSECTION		Queuing: 50 - 325 feet		R/W & Total Takes = 1.71 acres		

Roundabout – with ROW concessions



Traffic Models, Part II



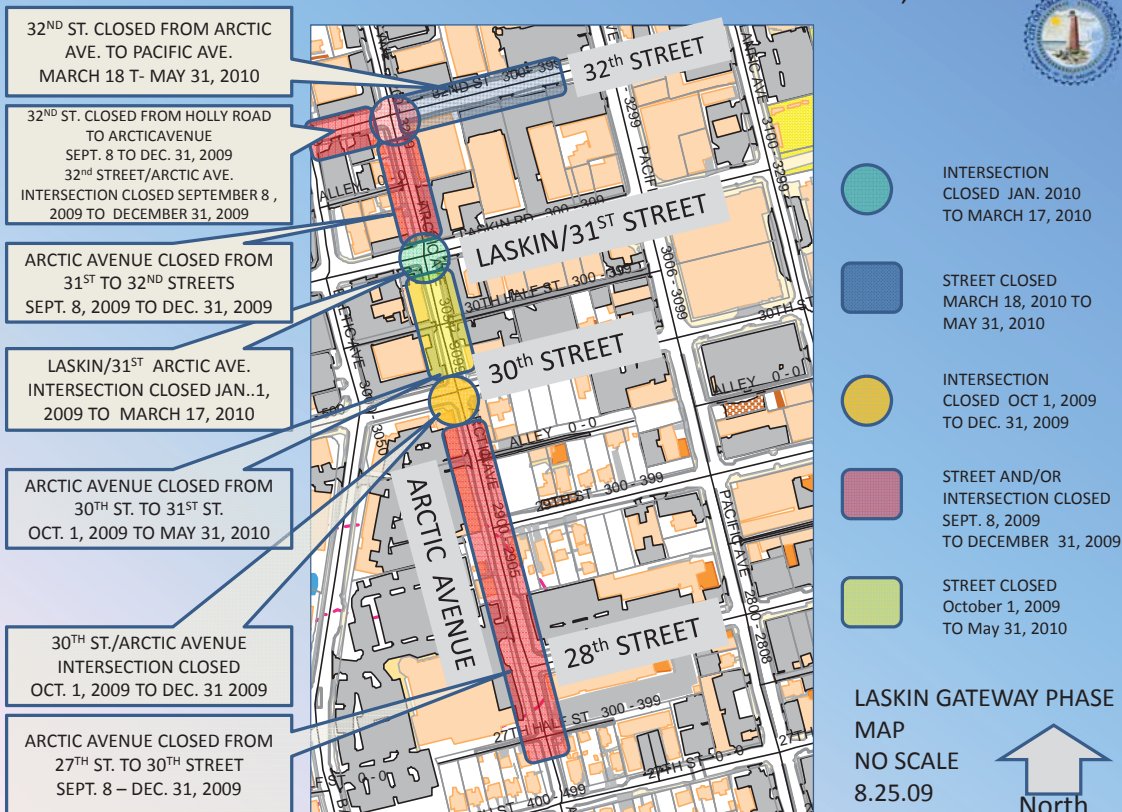
- Over 50 unique Synchro models developed.
- Multiple time of year scenarios (off season, shoulder season, peak season)
- Can we get some North End commuters to shift to 30th, this time, maybe?

2014_Total_Traffic_PM_Off_Season_2_WB_Lanes_ALL_3 LANES_SCENARIO_4A	10/7/2009 2:57 PM
2014_Total_Traffic_PM_Off_Season_Roundabout_4Ins_SCENARIO_1	10/7/2009 3:29 PM
2014_Total_Traffic_PM_Off_Season_Scenario_3A with Shift	10/7/2009 6:23 AM
2014_Total_Traffic_PM_Off_Season_Scenario_3A_without_Shift	10/7/2009 3:04 PM
2014_Total_Traffic_PM_Off_Season_Scenario_3A_without_Shift_SPEED racer	10/7/2009 3:41 PM

Selected Alternative



LASKIN GATEWAY PHASE 1 SEPTEMBER 8-2009 TO MAY 31, 2010



Questions?

